

It is the builder's complete responsibility to ensure that all plans submitted for approval fully comply with the Architectural Guidelines and all applicable regulations and requirements including zoning provisions and any provisions in the subdivision agreement. The Control Architect is not responsible in any way for examining or approving site (lotting) plans or working drawings with respect to any zoning or building code or permit matter or that any house can be properly built or located on its lot.

This is to certify that these plans comply with the applicable Architectural Design Guidelines approved by the Town of INNISFIL.

JOHN G. WILLIAMS LTD., ARCHITECT
ARCHITECTURAL CONTROL REVIEW
AND APPROVAL

APPROVED BY:

DATE: JUL 19, 2021

This stamp certifies compliance with the applicable Design Guidelines only and bears no further professional responsibility.

Town of Innisfil
Lot Grading & Zoning Approval

kbayley

Aug 04, 2021



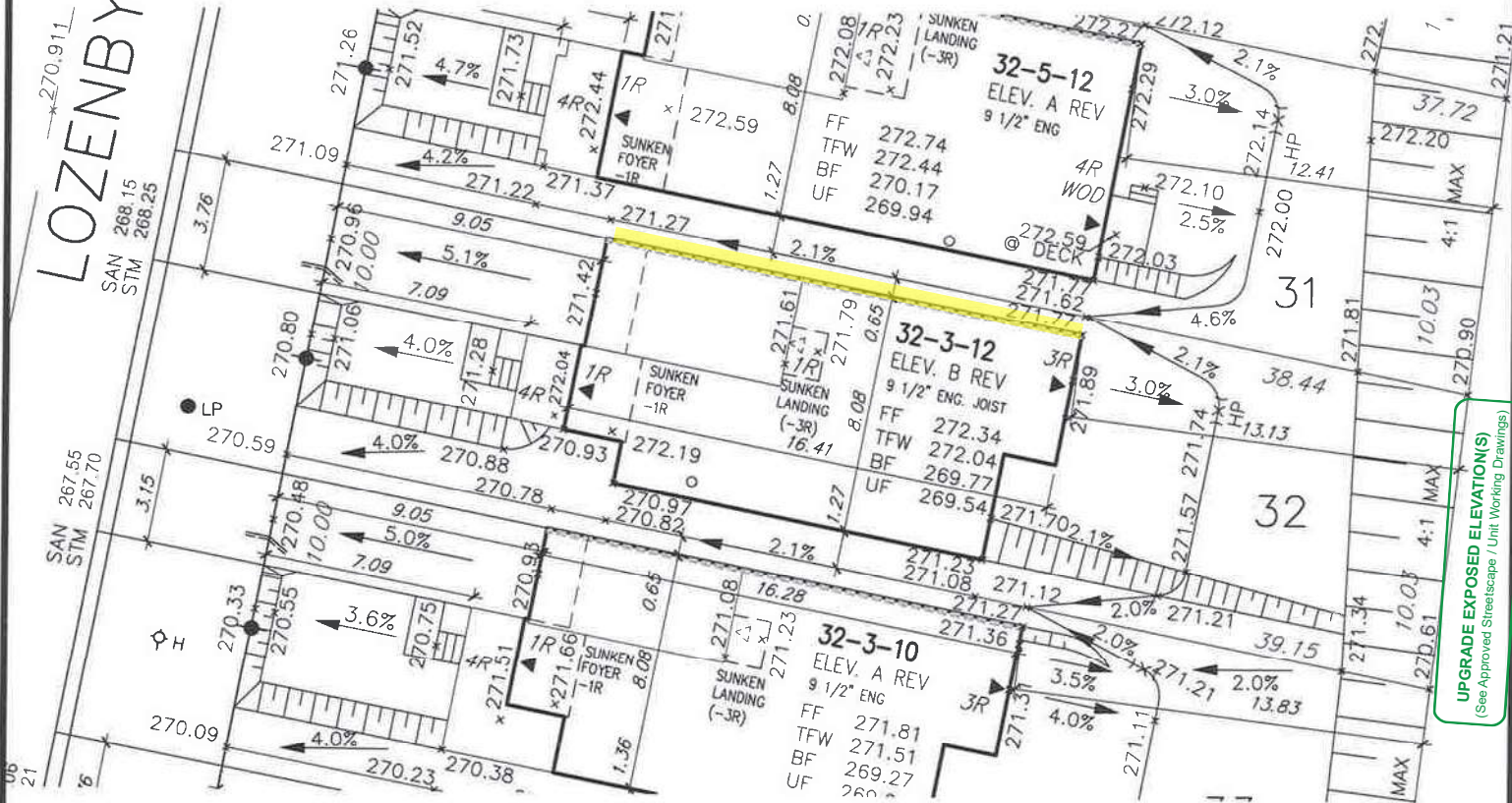
URBAN ECOSYSTEMS LTD. HAS REVIEWED THE PROPOSED GRADES AS SHOWN ON THIS PLAN AND HAS FOUND THEM TO BE IN GENERAL CONFORMANCE WITH THE APPROVED PLANS

LOT NO. 32

DATE July 16, 2021



LOZENBY ST.



UPGRADE EXPOSED ELEVATION(S)
(See Approved Streetscape / Unit Working Drawings)

ALL DIMENSIONS ARE IN METRIC
UNLESS OTHERWISE NOTED

COVERAGE					BLD. HEIGHT 9.0M (MAX)		MIN LANDSCAPE (30%)		
LOT NO.	MODEL	LOT AREA S.M.	UNIT COVERAGE S.M.	(MAX. 45%)	AVG. GRADE AROUND EXT WALL	HEIGHT	DRIVEWAY	LANDSCAPE	%
32	32-3-12 'B'	388.030 S.M.	138.11 S.M.	35.59 %	$(271.77+271.89+271.70+271.23+270.97+270.93+271.28+271.42)/8=271.40$	8.16	34.02	215.90	55.64

Grading Notes:

It shall be the responsibility of the builder to have all grades and services verified and checked before construction by an approved grading engineer. These grades and the placement of storm and sanitary services off the street are to meet the requirements of the municipality having jurisdiction.

VA3 Design Inc. is to be notified promptly of any discrepancies at least 1 (one) week prior to excavation commences in order that the building can be re-sited. Failure to observe these conditions may require expensive remedial action that will not be the responsibility of or cost to VA3 Design Inc. Foundation wall shall be poured to a minimum of 6" above approved grades.

Finished grade lines as indicated on the house prototype are for reference only and do not necessarily depict finish grading conditions of any particular lot.

1 ISSUED FOR CLIENT REVIEW

JUN 04-21 RC

2 REVISED AS PER ENGS COMMENTS

RC

3

- PROPOSED VALVE
- LP LIGHT POLE
- WATER SERVICE
- DOUBLE STM./SAN. CONNECTION
- SINGLE STM./SAN. CONNECTION
- CATCH BASIN
- CABLE TELEVISION PEDESTAL
- BELL PEDESTAL

- HYDRANT
- TRANSFORMER

- NO. OF RISERS
- FINISHED FLOOR ELEVATION
- FINISHED MAIN LEVEL ELEVATION
- UNDERSIDE FOOTING ELEVATION
- FIN. BASEMENT FLOOR SLAB
- TOP OF FOUNDATION WALL
- UNDERSIDE FOOTING AT REAR
- UNDERSIDE FOOTING AT FRONT
- UNDERSIDE FOOTING AT SIDE
- WALK OUT DECK
- W.O.B. WALK OUT BASEMENT
- REVERSE PLAN

- STREET SIGN
- MAIL BOX
- RETAINING WALL
- CHAIN LINK FENCE (SEE LANDSCAPE PLAN)
- ACOUSTICAL FENCE (SEE LANDSCAPE PLAN)
- WOOD SCREEN FENCE (SEE LANDSCAPE PLAN)
- HYDRO SERVICE LATERAL
- HYDRO METER
- GAS METER
- SWALE DIRECTION
- EMBANKMENT (3:1 SLOPE unless otherwise noted)

- PROVIDE 3/4" DIA. CLEAR STONE IN THIS AREA
- THIS LOT CONTAINS ENGINEERED FILL
- AIR CONDITIONER REQUIRED
- RAIN WATER DOWNSPOUT LOCATION (DISCHARGE ONTO SPLASHPAD)
- SIDE WINDOW LOCATION
- OPT. DOOR LOCATION
- EXTERIOR DOOR LOCATION
- REDUCE SIDE YARD



The undersigned has reviewed and takes responsibility for this design and has the qualifications and meets the requirements set out in the Ontario Building Code to be a Designer.

Wellington Jno-Baptiste 25591
name BCIN
registration information
VA3 Design Inc. 42658



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va3design.com

BAYVIEW WELLINGTON

project name
ALCONA SHORES

municipality
INNISFIL

lot/block no.
32

registered plan no.

project no.
13049

date
SEP 2017

drawn by
RC

checked by
RC

scale
1:250

SITE PLAN

file name
13049-SP-TAB-PH3

drawing no.
1

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